CONDITIONAL REZONING CONCEPT PLAN

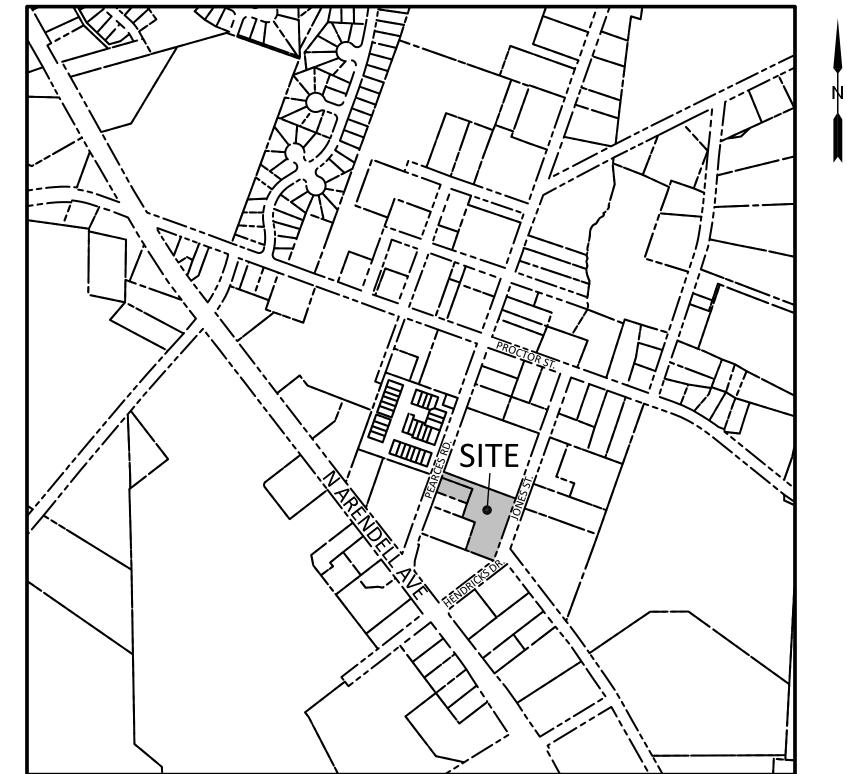
24/7 FITNESS CLUB

301 JONES STREET AND 108 PEARCES ROAD ZEBULON, NC 27597

NOVEMBER 1, 2023

REVISED JANUARY 18, 2024 REVISED MARCH 12, 2024

VICINITY MAP



CG ENTERPRISES OF NC LLC 1609 WHISPERING MEADOWS DRIVE ZEBULON, NC 27597 **CONTACT: CURTIS GINGLES** PHONE: 470-309-4479 EMAIL: CUR_GIN@MSN.COM

DEVELOPER

CG ENTERPRISES OF NC LLC 1609 WHISPERING MEADOWS DRIVE ZEBULON, NC 27597 **CONTACT: CURTIS GINGLES** PHONE: 470-309-4479 EMAIL: CUR_GIN@MSN.COM

CIVIL ENGINEER

O'KANE AND ASSOCIATES, PLLC 107 UNION DRIVE, SUITE 202 WASHINGTON, NC 27889 CONTACT: BRIAN O'KANE, PE PHONE: 252-702-1910 EMAIL: BRIAN@OKANEANDASSOCIATES.COM

SHEET INDEX				
DRAWING	<u>TITLE</u>	REVISION DATE		
C-0.0	COVER SHEET	03/12/2024		
C-1.0	EXISTING CONDITIONS PLAN	03/12/2024		
SITE LAYOUT AND UTILITY				
C-2.0	CONCEPTUAL SITE PLAN	03/12/2024		
C-3.0	CONCEPTUAL LANDSCAPE PLAN	03/12/2024		
ARCHITECTURAL ELEVATIONS (BY OTHERS PROVIDED FOR REFERENCE ONLY)				
A-201	ARCHITECTURAL ELEVATIONS	03/12/2024		

TOTAL NUMBER OF DRAWINGS INCLUDED IN SET = 5

SCALE: 1"=500'

LANDOWNER

DEVELOPER				
1609 WHISPERING	SES OF NC LLC MEADOWS DRIVE NC 27597			
LANDOWNER				
1609 WHISPERING	SES OF NC LLC MEADOWS DRIVE NC 27597			
EXISTING PROPERTY DATA				
SITE ADDRESS	301 JONES STREET AND 108 PEARCES ROAD			
PIN	2706-01-5099 & 2706-01-4224			
PLANNING JURISDICTION	TOWN OF ZEBULON			
PARCEL AREA	0.312 + 1.603 = 1.915 ACRES			
EXISTING ZONING	R2			
EXISTING LAND USE	VACANT			
CURRENT DEED	DB 19356 PG 637			
FEMA FLOOD INSURANCE MAP	3720270600K (07-19-2022)			
FEMA FLOODZONE	ZONE "X"			
RIVER BASIN	NEUSE			
RECEIVING STREAM	UNNAMED TRIBUTARY TO LITTLE RIVER (TARPLEYS POND) STREAM INDEX - 27-57-(8.5) CLASSIFICATION - WS-V;NSW			

EXISTING ZONING	R2			
PROPOSED ZONING	GC-C			
EXISTING SITE AREA	1.915 ACRES			
R/W DEDICATION	0.06± ACRES			
OTHER DEDICATION	0 ACRES			
PROPOSED NET SITE AREA	1.855 ACRES			
PROPOSED USE	GYMANSIUM & RETAIL			
DEVELOPMENT STANDARDS				
MAX. RESIDENTIAL DENSITY	7 UNITS / ACRE			
PROPOSED RES. DENSITY	0 UNITS / ACRE			
MIN. LOT AREA	6,000 SF			
PROPOSED LOT AREA	80,803± SF			
MIN. LOT WIDTH	50 LF			
PROPOSED LOT WIDTH	111± LF			
MAX. LOT COVERAGE	80%			
PROPOSED LOT COVERAGE	14±%			
MIN. STREET SETBACK	30 FEET			
MIN. SIDE SETBACK	5 FEET			
MIN. REAR SETBACK	25 FEET			
MAX. BUILDING HEIGHT	50 FEET			
PROPSOED BUILDING HEIGHT	LESS THAN 50 FEET			
REQUIRED OPEN SPACE	3.0%			
REQUIRED OPEN SPACE	2,500 SF			
PROVIDED OPEN SPACE	2,502 SF			
REQUIRED PARKING	1 SPACE / 200 SF (FITNESS CENTER)			
	1 SPACE / 200 SF (RETAIL)			
REQUIRED PARKING	17,823 NET SF FITNESS / 200 =			
	89 SPACES			
	3,762 NET SF RETAIL / 200			
	18 SPACES			
REQUIRED PARKING	99 SPACES			
PROVIDED PARKING	76 SPACES			
REQUIRED ADA PARKING	3 SPACES			
PROVIDED ADA PARKING	3 SPACES			
BICYCLE PARKING REQUIRED	108 SPACES / 20 = 6 SPACES			
BICYCLE PARKING PROVIDED	6 SPACES			

SITE INFORMATION TABLE

ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE TOWN OF ZEBULON STANDARDS AND SPECIFICATIONS

PRELIMINARY DRAWING NOT RELEASED FOR CONSTRUCTION

REVISIONS

REVISED PER TOWN OF ZEBULON 01-18-2024 REVISED PER TOWN OF ZEBULON 03-12-2024

ENTERPRISES

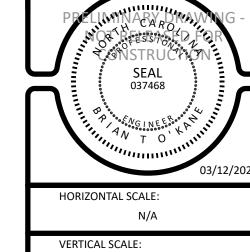
CONDITIONAL REZONING CONCEPT PLAN

FITNE

SHEET







DATE: 03/12/2024

SHEET NUMBER: C-0.0

CI- CURB INLET C/L- CENTERLINE CL- CLASS CM- CONCRETE MONUMENT CMP- CORRUGATED METAL PIPE CO-CLEAN OUT CONC- CONCRETE CONN - CONNECTION CP- CORRUGATED PLASTIC CY-CUBIC YARD DB- DEED BOOK DIP- DUCTILE IRON PIPE D.M.P- DOUBLE MERIDIAN DISTANCES DR-DRIVE DS- DOWNSP E- ELECTRIC EA - EACH E BOX- ELECTRICAL BOX ELEC- ELECTRIC **EP-EDGE OF PAVEMENT** E/S- EDGE OF SHOULDER EX- EXISTING F/C- FACE OF CURB FDC- FIRE DEPARTMENT CONNECTION FF- FINISHED FLOOR FH- FIRE HYDRANT F/L- FLOW LINE FL- FIRE LANE G-GAS GI- GRATE INLET GND- GROUND GTS- GAS TEST STATION GV- GAS VALVE

HORIZ- HORIZONTAL

IPF- IRON PIPE FOUND

IRF-IRON ROD FOUND

MECH- MECHANICAL

IPS- IRON PIPE SET

LF- LINEAR FEET

MH- MANHOLE

INV- INVERT

CONDITIONING

(M)- MEASURED METES & BOUNDS

ABBREVIATIONS:

B/C- BACK OF CURB

B/W - BOTTOM OF WALL

CG- CURB AND GUTTER

B/L- BASE LINE

CB- CATCH BASIN

AFG- ABOVE FINISHED GRADE

APPR LOC. - APPROXIMATE LOCATION

PB- PLAT BOOK CBR- CALIFORNIA BEARING RATIO PC- POINT OF CURVATURE PED-PEDESTAL PG-PAGE PH-PHASE PI- POINT OF INTERSECTION PKG-PARKING P/L- PROPERTY LINE PT-POINT OF TANGENT PP-POWER POLE PVC- POLYVINYL CHLORIDE PVMT- PAVEMENT PWR-POWER (R)- RECORDED METES & BOUNDS RD-ROOF DRAIN **RJ- RESTRAINED JOINT** R/W - RIGHT-OF-WAY RCP- REINFORCED CONCRETE PIPE S-SLOPE SAN-SANITARY SEWER SDWK-SIDEWALK SF- SILT FENCE SPT-SPOT GRADE SS- SANITARY SEWER CONNECTION STA - STATION STD-STANDARD STM-STORM STMH- STORM SEWER MANHOLE T- TELEPHONE TBC-TOP BACK OF CURB TCM-TELECOMMUNICATIONS MANHOLE T/C- TOP OF CURB TEL- TELEPHONE UGE- UNDERGROUND ELECTRIC UNK- UNKNOWN **UP- UTILITY POLE** HDPE- HIGH DENSITY POLYETHYLENE VAR- VARIABLE VCP- VITRIFIED CLAY PIPE HVAC- HEATING, VENTILATION, & AIR VERT- VERTICAL

W/- WITH

W - WATER

W/L- WATER LINE

X-ING- CROSSING

Ø - DIAMETER

YI- YARD INLET

WUS- WATERS OF THE US

MIN- MINIMUM

MIN S- MINIMUM SLOPE

MJ- MECHANICAL JOINT

OHE- OVERHEAD ELECTRIC

OHT- OVERHEAD TELEPHONE

OHP- OVERHEAD POWER

NIC- NOT IN CONTRACT

NTS- NOT TO SCALE

- 1. SITE PLAN APPROVAL FROM THE TOWN OF ZEBULON IS REQUIRED FOR THIS DEVELOPMENT.
- 2. ALL REQUIRED IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE TOWN OF ZEBULON STREET AND STORM DRAINAGE STANDARDS AND SPECIFICATIONS MANUAL.
- 3. ALL DEVELOPMENT SHALL COMPLY WITH THE REQUIREMENTS OF THE TOWN OF ZEBULON UNIFIED DEVELOPMENT ORDINANCE (UDO)
- 4. PRIOR TO CONSTRUCTION THE CONTRACTOR SHALL CONFIRM THAT ALL REQUIRED PERMITS, EASEMENT, RIGHT-OF-WAY, AND ENCROACHMENT AGREEMENTS HAVE BEEN OBTAINED.
- 5. THE LAND SURVEYOR RESPONSIBLE FOR STAKING THE IMPROVEMENTS SHALL INDEPENDENTLY VERIFY THE HORIZONTAL AND VERTICAL DATUM FOR THIS PROJECT PRIOR TO CONSTRUCTION.
- 6. REFER TO ARCHITECTURAL AND STRUCTURAL DESIGN PLANS FOR THE BUILDING DETAILS AND DIMENSIONS.
- 7. ALL GRADES, SLOPES TO BUILDING ACCESS/EGRESS POINTS, HANDICAP RAMPS, STRIPING AND PAVEMENT MARKINGS SHALL BE INSTALLED IN ACCORDANCE WITH ADA REQUIREMENTS AND THE NORTH CAROLINA

DESIGN STANDARD NOTES

- 1. THIS PROJECT WILL CONNECT TO THE CITY OF RALEIGH PUBLIC WATER
- 2. THIS PROJECT WILL CONNECT TO THE CITY OF RALEIGH PUBLIC SEWER
- 3. THIS PROJECT LIES WITHIN THE TOWN OF ZEBULON'S PLANNING AND ZONING JURISDICTION AND IS SUBJECT TO THE REGULATIONS SI FORTH IN THE TOWN OF ZEBULON UNIFIED DEVELOPMENT ORDINANCE (UDO).
- 4. THIS PROJECT WILL DISTURB MORE THAN 1 ACRE OF LAND AND WILL REQUIRE A LAND DISTURBANCE PERMIT FROM WAKE COUNTY.
- 5. THIS PROJECT WILL REQUIRE A STORMWATER PERMIT FROM WAKE
- 6. ANY WORK PERFORMED WITHIN THE NCDOT RIGHT-OF-WAY WILL REQUIRE THE APPROVAL OF AN ENCROACHMENT AGREEMENT PRIOR TO CONSTRUCTION.

UTILITY ALLOCATION POLICY COMPLIANCE:

SINGLE USE RETAIL- 40 POINTS
- REFER TO SHEET C-2.0

BONUS POINTS:

CATEGORY 2-SECTION 2B- PARKING PROVISIONS OF ON-STREET PUBLIC PARKING- 3 STALLS- 3 POINTS

INSTALLATION OF 3 ON-STREET PARKING STALLS REFER TO SHEET C-2.0 SECTION 2C- STORMWATER SCMS

STORMWATER-BIORETENTION-5 POINTS INSTALLATION OF BIORETENTION FOR STORMWATER CONTROL REFER TO SHEET C-2.0

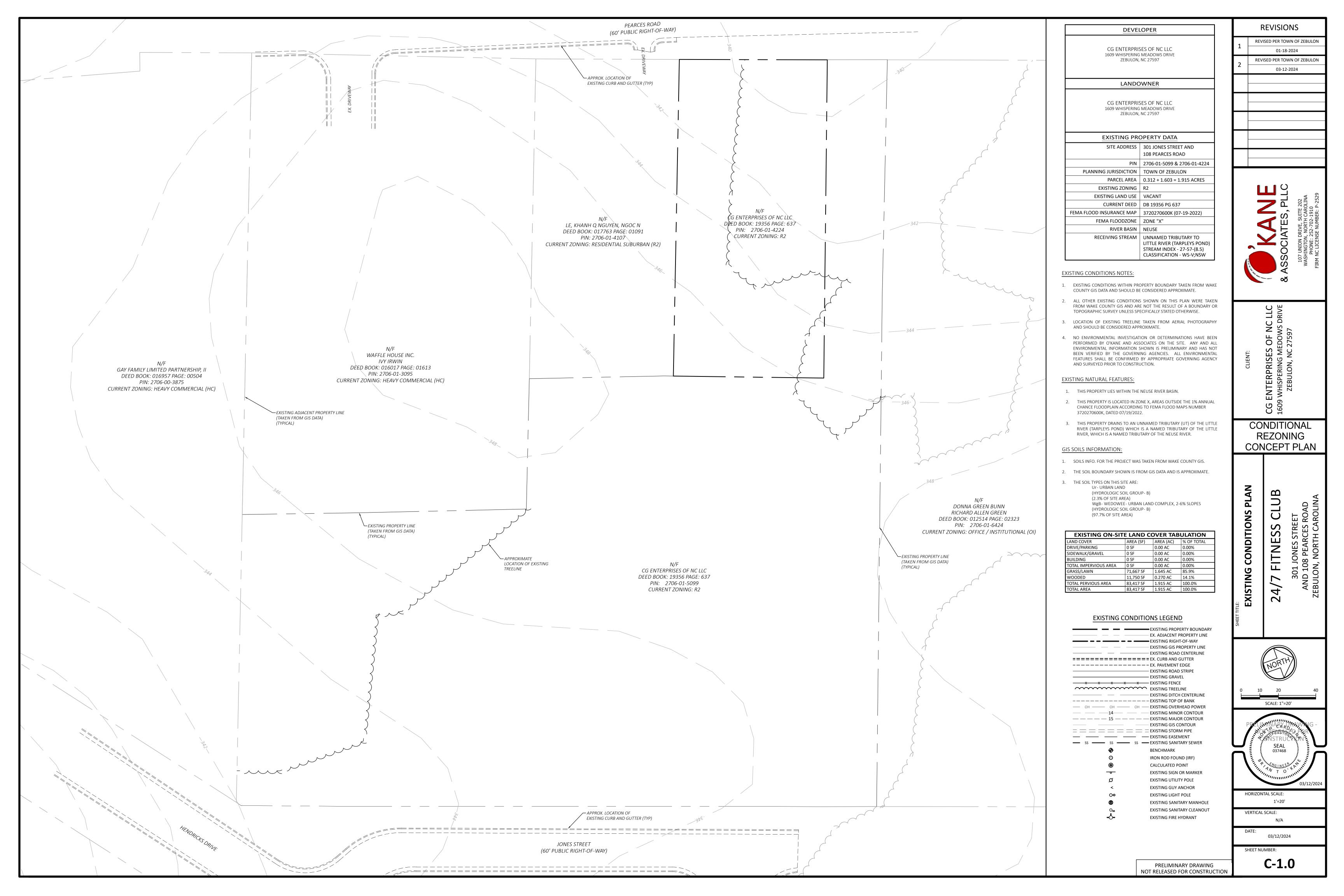
SECTION 2D-BUILDING / SITE DESIGN PEDESTRIAN ORIENTED AND WALKABLE SITE DESIGN- 5 POINTS INSTALLATION OF SIDEWALK FROM JONES STREET TO PEARCES REFER TO SHEET C-2.0

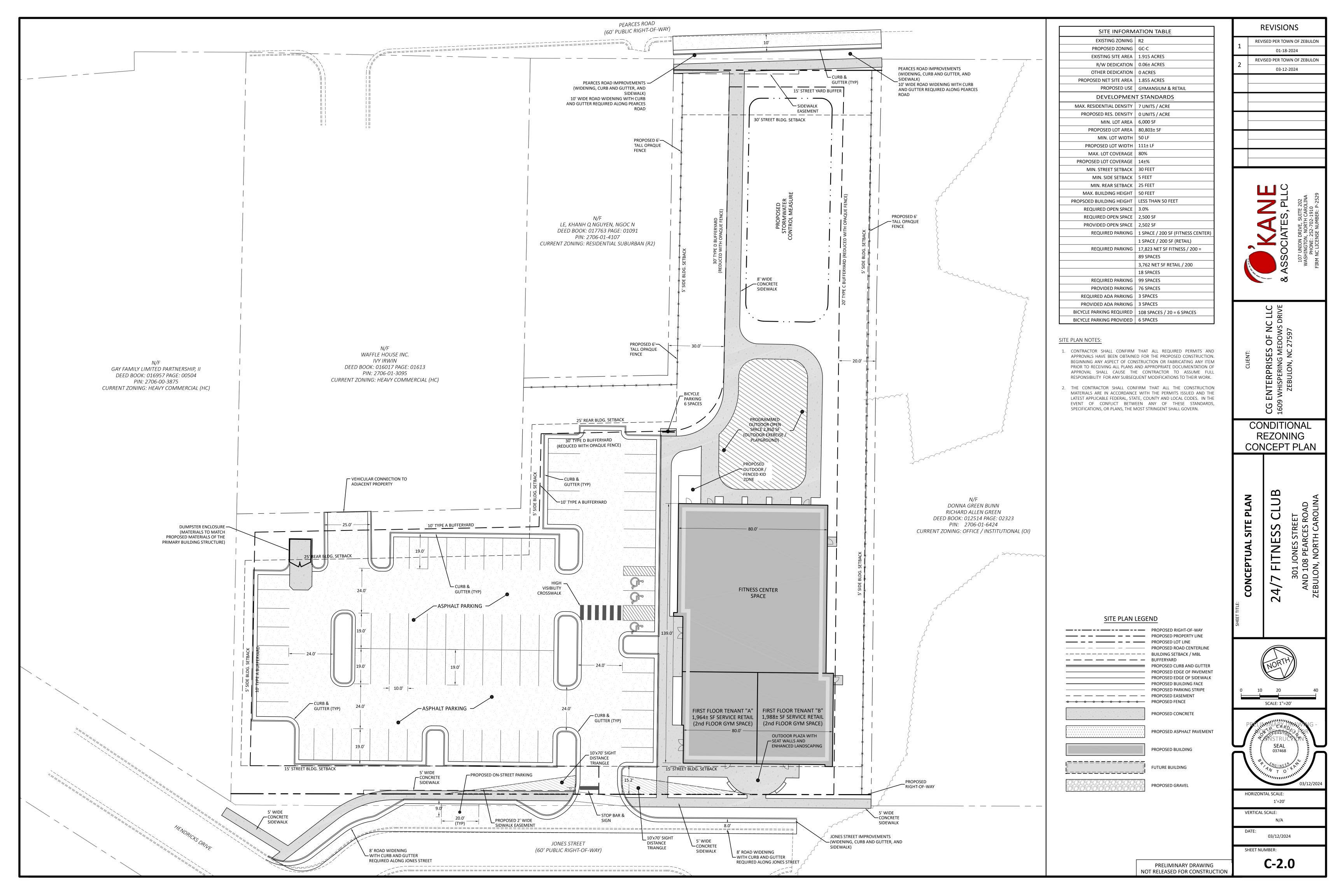
SECTION 3A- OUTDOOR ENHANCEMENT INSTALLATION OF NATIVE SHADE TREE SPECIES- 10 POINTS
- A MINIMUM OF 10 SHADE TREES SHALL BE NATIVE IN SPECIES REFER TO SHEET C-3.0

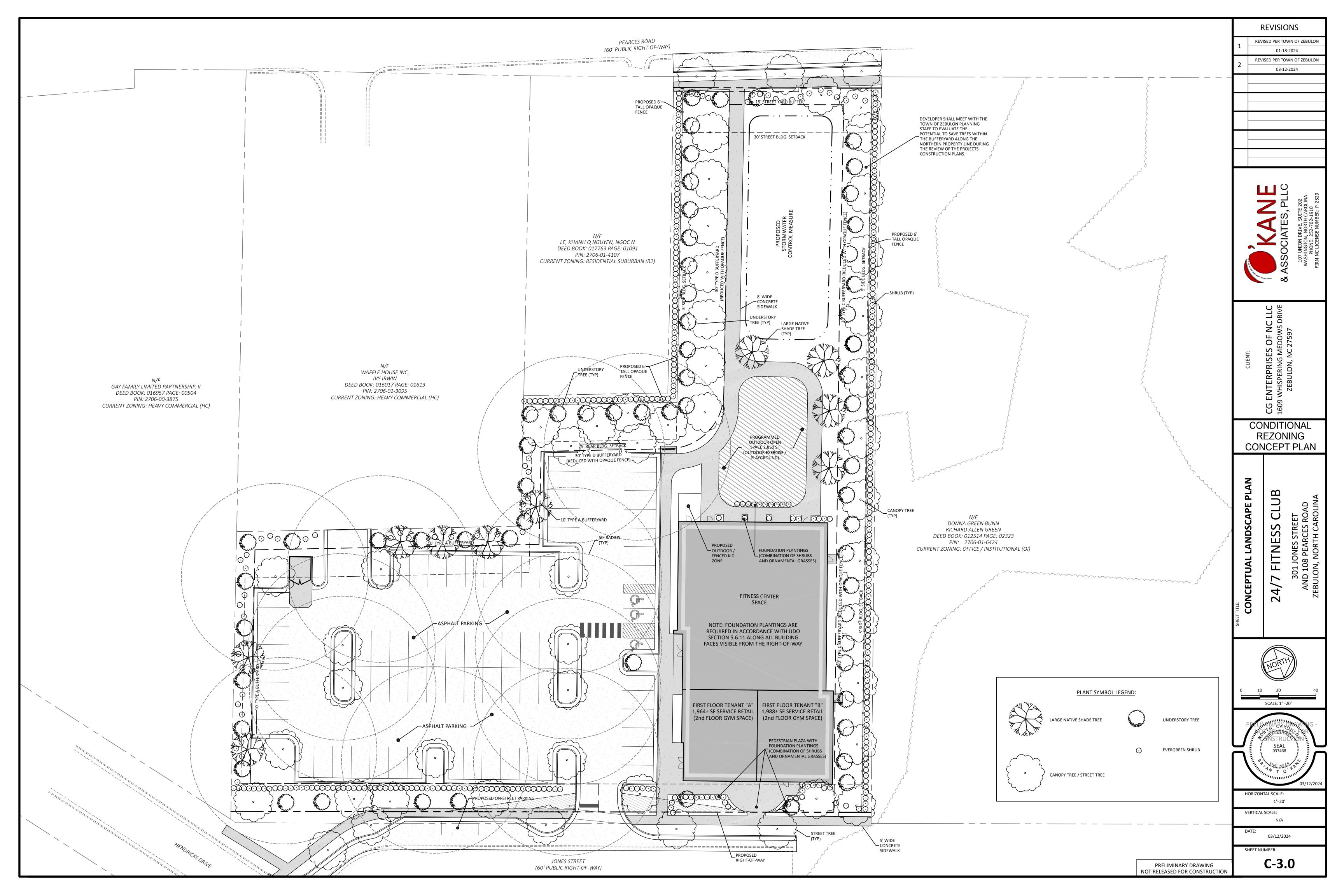
TOTAL POINTS = 40 + 3 + 5 + 5 + 10 = 63 POINTS

REZONING CONDITIONS:

- 1. THE STEM LENGTH OF THE PARKING LOT CONNECTION TO JONES STREET WILL BE AS SHOWN ON CONDITIONAL REZONING SITE PLAN (SHEET C-2.0) AND DOES NOT MEET THE 75' MINIMUM LENGTH SET FORTH IN UDO SECTION 5.1.6.F.
- 2. THE TOTAL NUMBER OF PARKING SPACES PROVIDED FOR THE PROJECT IS 79 WHICH IS LESS THAN THE REQUIRED NUMBER OF SPACES REQUIRED IN TABLE 5.8.4.H OF THE UDO.
- 3. MODIFIED STREET BUILDING SETBACK ALONG JONES STREET TO 15'.
- 4. MODIFIED STREET YARD BUFFER BETWEEN PROPOSED BUILDING AND JONES STREET TO PROVIDE PEDESTRIAN ORIENTED PLAZA CONNECTION TO BUILDING.









A2 SOUTH (PRIMARY)
A-201 1/8" = 1'-0"

A1 RENDERING (SE PERSPECTIVE)

A-201 NOT TO SCALE

3.15.24